

Approved: June 3, 2021

Ordinance #2021-6-3-1

TOWN OF BALDWIN ROAD ORDINANCE CONSTRUCTION, DESIGN, LAYOUT & FINANCIAL ASSURANCE STANDARDS

A. ROAD DESIGN

1. DESIGN OBJECTIVES & JURISDICTION

- a. The road system shall be designed to meet the following objectives: to permit the safe, efficient, and orderly movement of traffic; to meet the needs of the present and future population with a simple and logical pattern; to respect natural features and topography; minimize local road maintenance and replacement costs; minimize private roads and private residential driveway accesses to public roads; and to present an attractive appearance.
- b. In the course of a subdivision review, the **Town Board** shall designate roads as arterial, collector, subcollector, or access roads. This decision shall be based upon State, County or town comprehensive plans or official maps.
- c. The **Town Board** may require any road to be constructed to the boundary of the subdivision.
- d. The **Town Board** may require special setbacks, screening and other buffers within the road right of way and may limit access.
- e. The road standards in this section for both Conventional Development and Conservation Design Development may be applied to Conservation Design Development as provided for in St. Croix County's Land Division Ordinance, §§ 13.2 C. and 13.7 K. The road standards in this section for Conservation Design Development may only be applied to Conservation Design Development or in specific instances approved by the **Town Board**.
- f. All road rights-of-way that are included within the design of a subdivision and that serve three or more lots shall be offered for dedication and accepted by the town. Once dedicated, the town controls vegetation within the right-of-way.
- g. The town must agree to accept the responsibility for maintenance of the public road and road-related facilities before plat approval and construction.

2. DESIGN & CONSTRUCTION STANDARDS

- a. The minimum standards in this section shall apply to all roads, however, per Wisconsin Statute Ch. 82, the Town may modify these standards subject to Wis. Stat. §82.5 (2) and (3).
- b. Roads shall be designed and constructed in accordance with the minimum standards in the following tables or to standards set by the town:

RURAL ROAD CONSTRUCTION STANDARDS				
ROAD TYPE	Arterial & Collector Roads	Subcollector & Access Roads	Conservation Design Access Road	One-Way Access Road Conventional & Conservation Design
Traffic Lanes	2	2	2	1 ¹
R.O.W.	80'	66'	50'	50'
Road Width Before Gravel or Base Course	34'	31'	29'	26'
Road Width after Gravel or Base Course	30'	27'	25'	23'
Road Surface, excluding shoulders	24'	22'	20'	18'
Shoulders, paved or gravel	3'	2'	2'	2'
Culverts	18'' ²	18'' ²	18'' ²	18'' ²
Compacted Sand Sub Base ³	12''	12''	12''	12''
Compacted Base ³				
Crushed Limestone or	6''	6''	6''	6''
Wisconsin Grade #2 Gravel	7''	7''	7''	7''
Shoulder Slopes & Fill Slopes on fills to 3'	4:1 desirable, 3:1 maximum	4:1 desirable, 3:1 maximum	4:1 desirable, 3:1 maximum	4:1 desirable, 3:1 maximum
Shoulder Slopes & Fill Slopes below top 3'	2:1 maximum	2:1 maximum	2:1 maximum	2:1 maximum
Back Slopes	4:1 desirable, 3:1 average, 2:1 maximum	4:1 desirable, 3:1 average, 2:1 maximum	4:1 desirable, 3:1 average, 2:1 maximum	4:1 desirable, 3:1 average, 2:1 maximum
Minimum Radius of Curvature in ft. from Centerline for Deflections of 7° or More ⁴	300	200	100	100
Maximum Grade ⁴	8%	10%	10%	10%
Maximum Grade within 50' of the Center of an Intersection	2%	2%	2%	2%
Corner Radii ⁴	30'	30'	30'	30'
Town Paving Standard	4" Lift	3" Lift	2" Lift	2" Lift

¹ Counter clockwise movement.

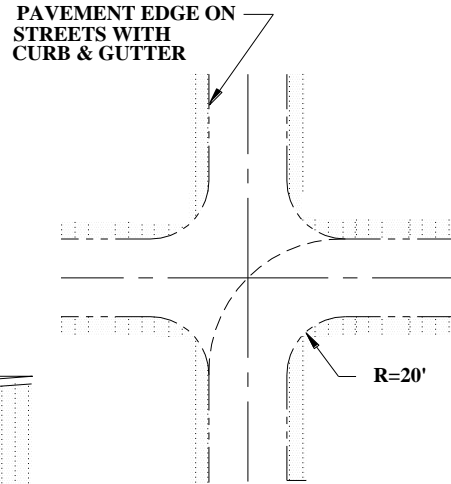
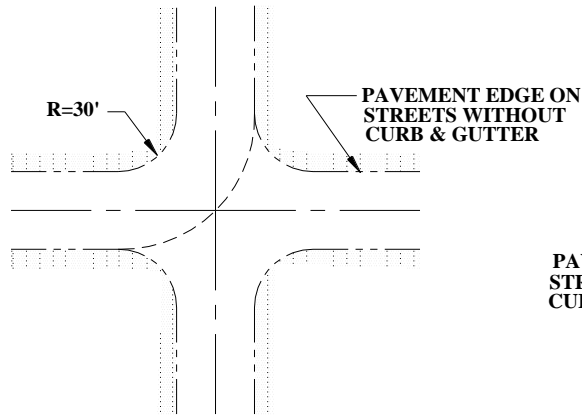
² Or as otherwise specified, with a minimum cover of one foot to the top of the sand lift. All culverts shall be galvanized, corrugated steel pipe, pipe arch, plate, or reinforced concrete pipe in conformity with American Association of State Highway Transportation Officials' (AASHTO) specifications.

³ Decomposable materials shall not be used in construction.

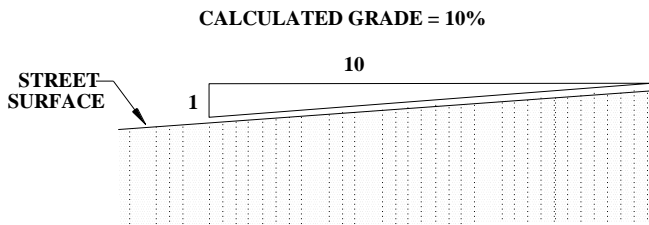
⁴ See diagrams below.

Town of Baldwin Ordinance #2021-6-3-1

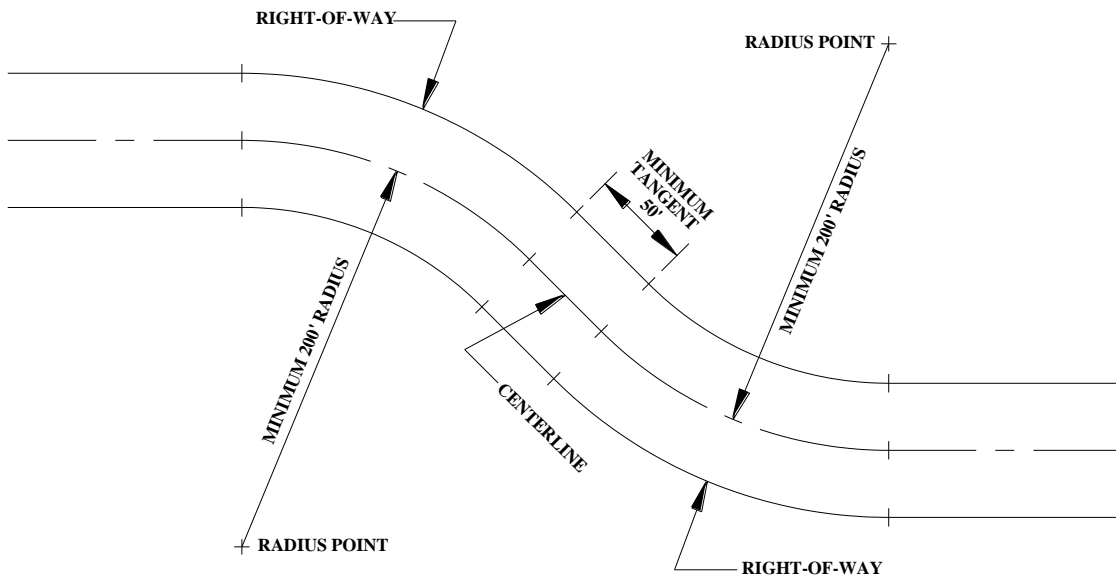
CORNER RADII

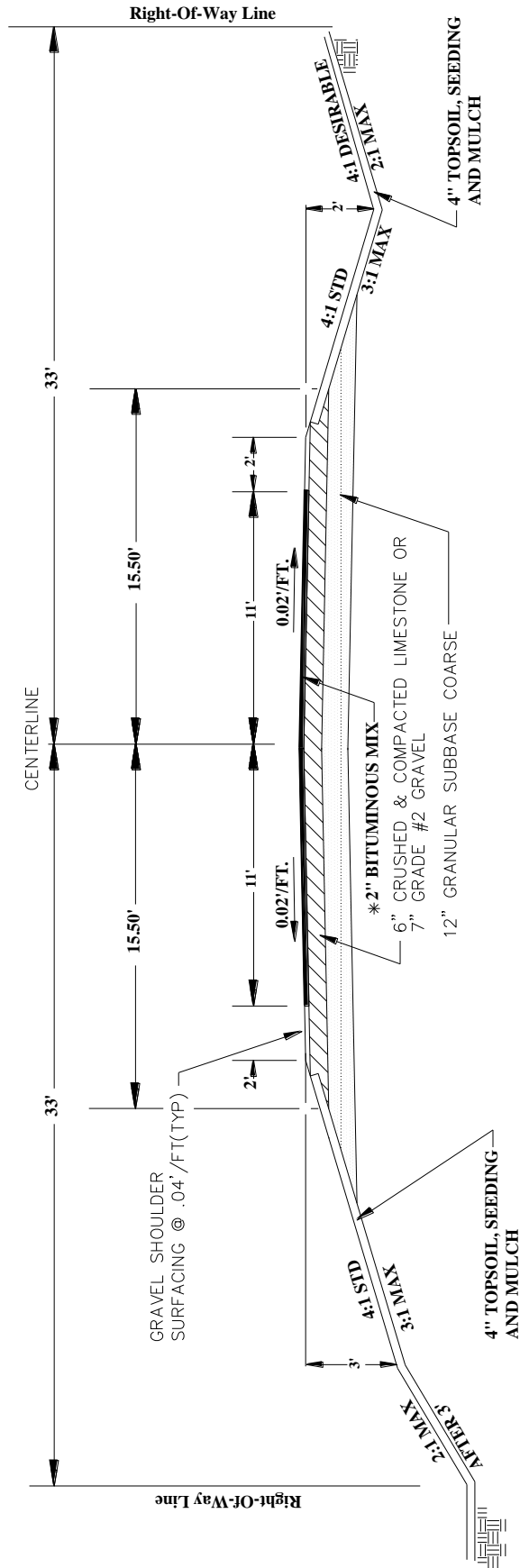


GRADES



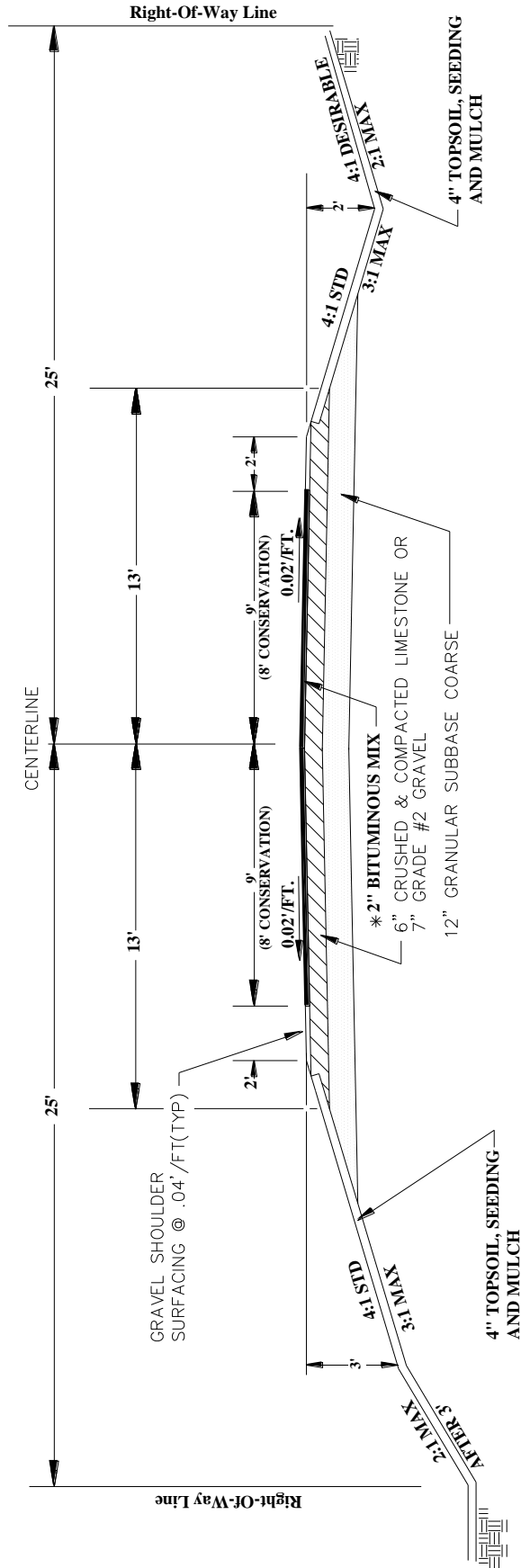
MINIMUM ROAD ALIGNMENT STANDARDS FOR CURVES AND TANGENTS





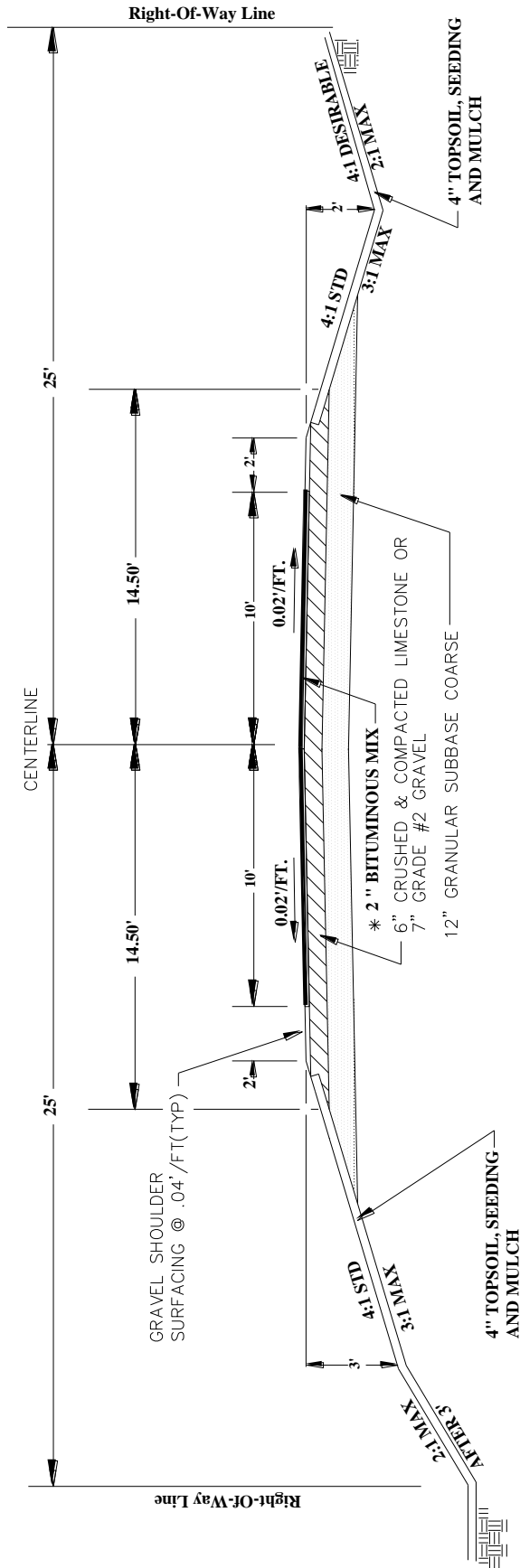
* EXACT PAVEMENT WIDTHS AND THICKNESSES MAY BE ADJUSTED BY SPECIFIC TOWNSHIP STANDARDS.

TYPICAL FINISHED SECTION STANDARD SUBCOLLECTOR AND ACCESS ROADWAY



* EXACT PAVEMENT WIDTHS AND THICKNESSES MAY BE ADJUSTED BY SPECIFIC TOWNSHIP STANDARDS.

TYPICAL FINISHED SECTION ONE WAY ACCESS ROAD & LOOP ROAD



* EXACT PAVEMENT WIDTHS AND THICKNESSES MAY BE ADJUSTED BY SPECIFIC TOWNSHIP STANDARDS.

TYPICAL FINISHED SECTION CONSERVATION DESIGN ACCESS ROAD

RURAL CUL-DE-SAC BULB AND LOOP END STANDARDS						
Standards	Conventional Development			Conservation Design Development		
ROAD TYPE	Cul-de-sac without Island	Cul-de-sac with Island	Loop Road	Cul-de-sac without Island	Cul-de-sac with Island	Loop Road
Traffic Lanes	2	1	1	1	1	1
One-Way Counter Clockwise Movement	NA	Yes	Yes	Yes	Yes	Yes
Vegetated Center Island¹	No	Yes	Yes	No	Yes	Yes
R.O.W.	60' radius	80' radius	50' width or 80' radius	50' radius	60' radius	50' width or 80' radius
Outside Pavement Radius, edge of pavement or face of curb	49'	69'	64'	35'	47'	63'
Center Island Radius, edge of pavement or face of curb	NA	47'	46'	NA	17'	47'
Road Width –Radius- Before Gravel or Base Course	54'	74'	69'	40'	52'	68'
Road Width –Radius- After Gravel or Base Course	51'	71'	66'	37'	34'	20'
Road Surface, no curb excluding shoulders	98'	22'	18'	70'	30'	16'
Paved Road Surface curbed			20'	Contact St. Croix County Highway Dept.		20'
Shoulders paved or gravel (no shoulder necessary with curb²)	2'	2'	2'	2'	2'	2'
Culverts	18" ³	18" ³	18" ³	18" ³	18" ³	18" ³
Shoulder Slopes & Fill Slopes on fills to 3'	4:1 desirable 3:1 maximum	4:1 desirable 3:1 maximum	4:1 desirable 3:1 maximum	4:1 desirable 3:1 maximum	4:1 desirable 3:1 maximum	4:1 desirable 3:1 maximum
Shoulder Slopes & Fill Slopes below top 3'	2:1 maximum	2:1 maximum	2:1 maximum	2:1 maximum	2:1 maximum	2:1 maximum
Back Slopes	4:1 desirable 3:1 average 2:1 maximum	4:1 desirable 3:1 average 2:1 maximum	4:1 desirable 3:1 average 2:1 maximum	4:1 desirable 3:1 average 2:1 maximum	4:1 desirable 3:1 average 2:1 maximum	4:1 desirable 3:1 average 2:1 maximum
Minimum Radius of Curvature in feet from Centerline for Deflections of 7° or More⁴	NA	NA	100	NA	NA	100
Maximum Grade⁴	6%	6%	6%	6%	6%	6%
Corner Radii⁴	30'	69'	30'	30'	30'	30'
Town Paving Standard	3" Lift	3" Lift	3" Lift	2" Lift	2" Lift	2" Lift

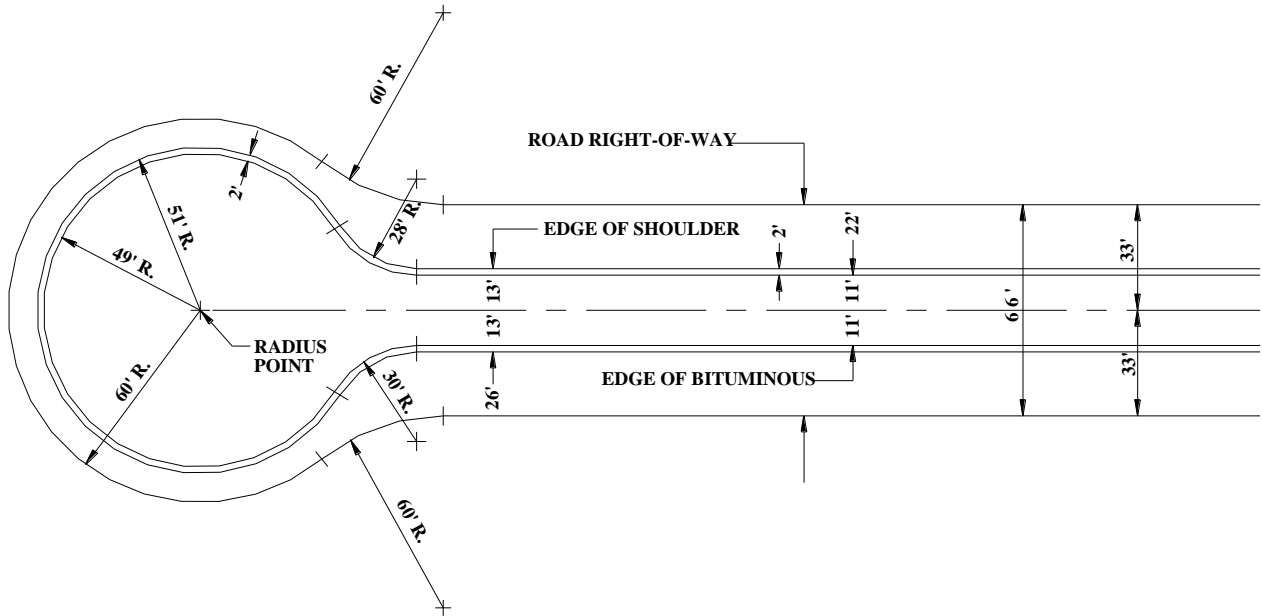
¹ See Landscape Screening, St. Croix County Chapter 13 Land Division and Chapter 15 Zoning.

² Mountable or surmountable concrete curb optional.

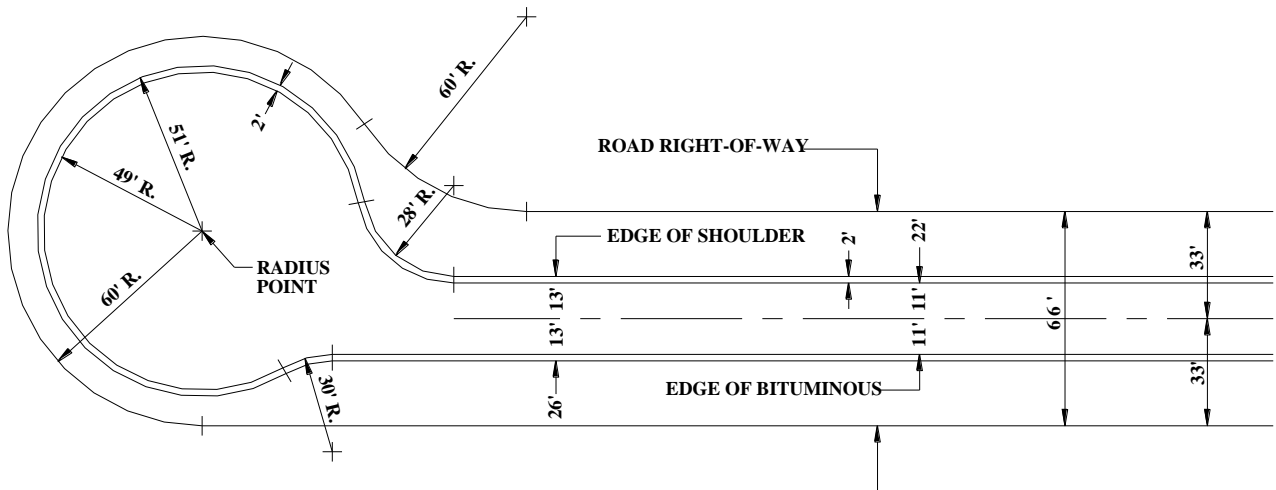
³ Or as otherwise specified, with a minimum cover of one foot to the top of the sand lift. All culverts shall be galvanized, corrugated steel pipe, pipe arch, plate, or reinforced concrete pipe in conformity with American Association of State Highway Transportation Officials' (AASHTO) specifications.

⁴ See diagrams above.

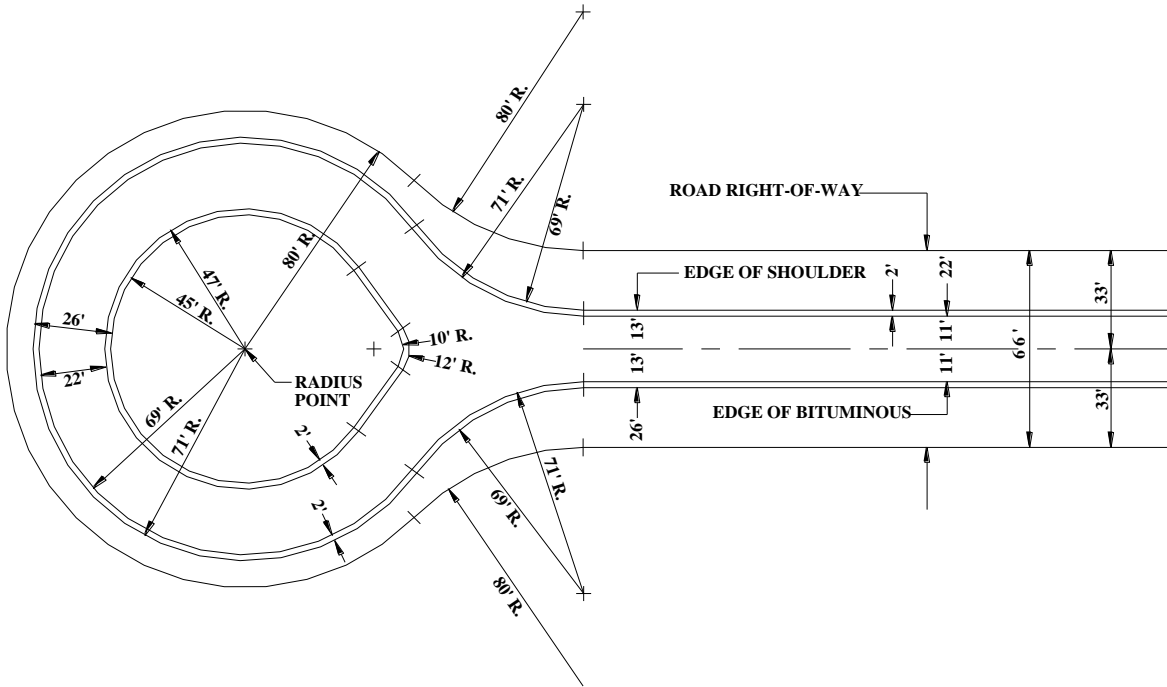
CONVENTIONAL DEVELOPMENT TYPICAL NON-ISLAND CUL-DE-SAC



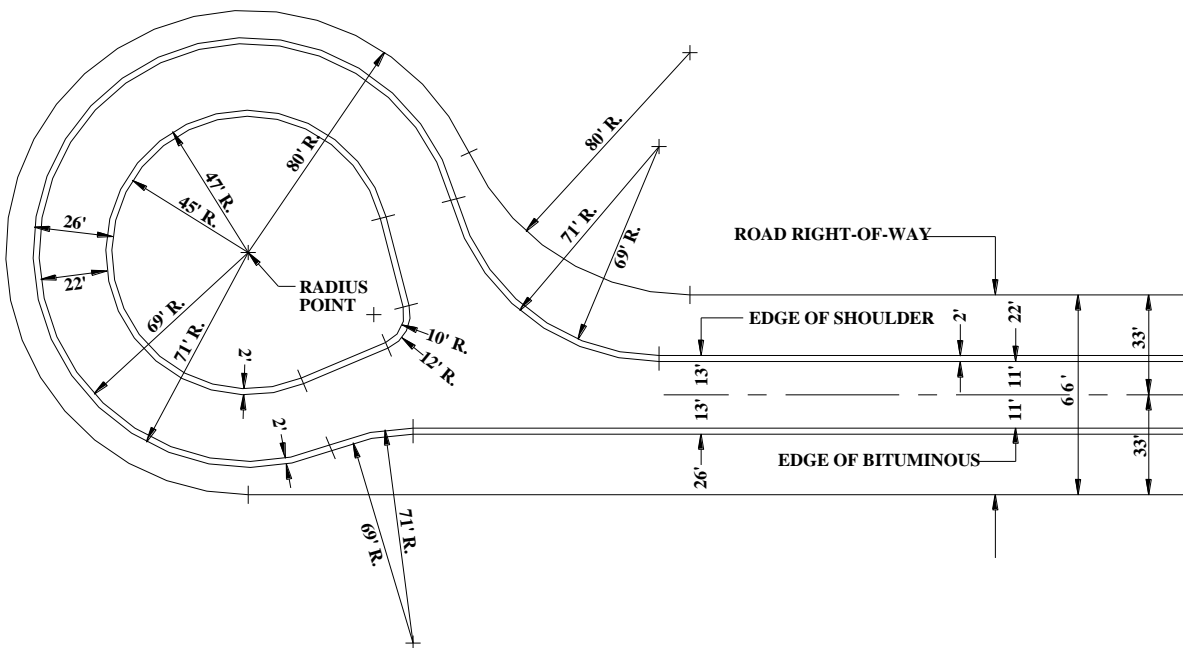
CONVENTIONAL DEVELOPMENT OFFSET NON-ISLAND CUL-DE-SAC



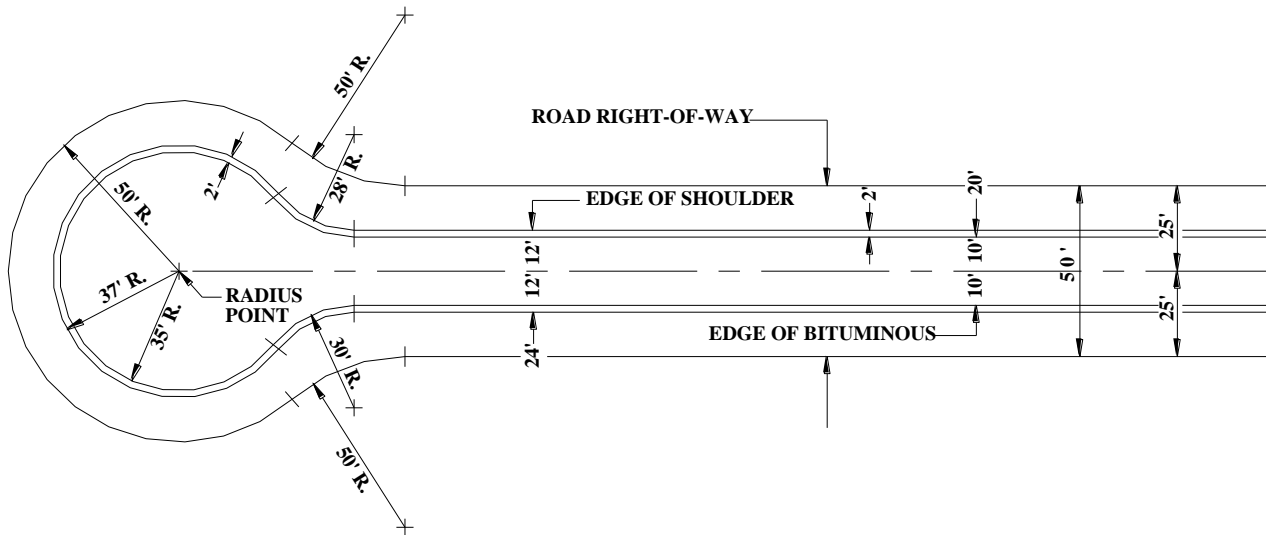
CONVENTIONAL DEVELOPMENT TYPICAL ISLAND CUL-DE-SAC



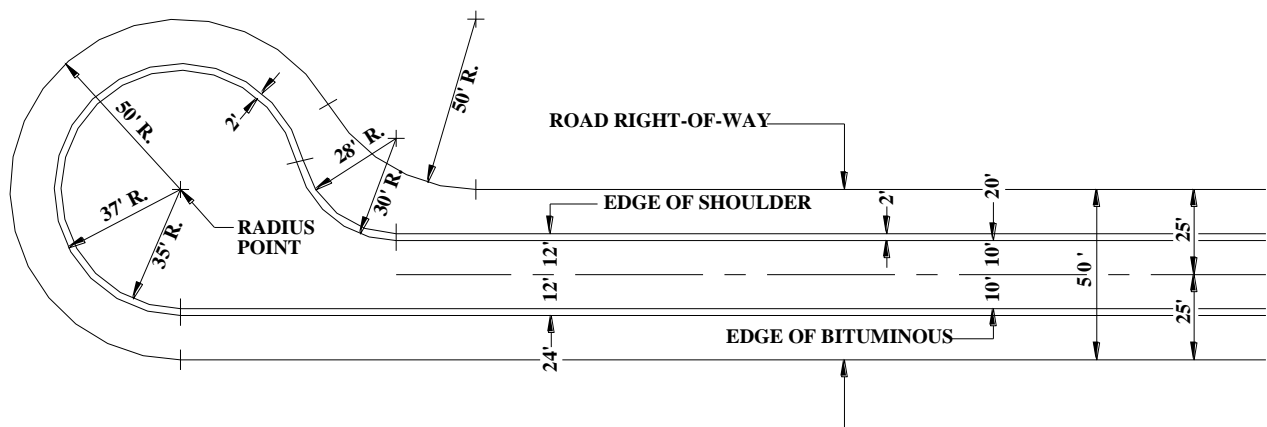
CONVENTIONAL DEVELOPMENT OFFSET ISLAND CUL-DE-SAC



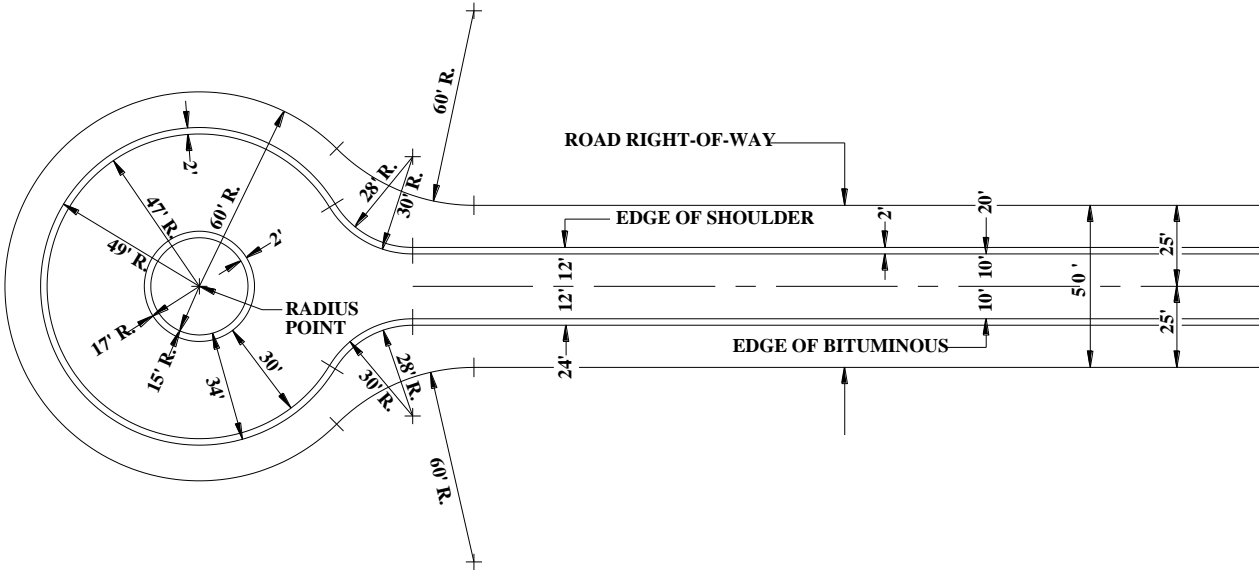
CONSERVATION DESIGN TYPICAL NON-ISLAND CUL-DE-SAC



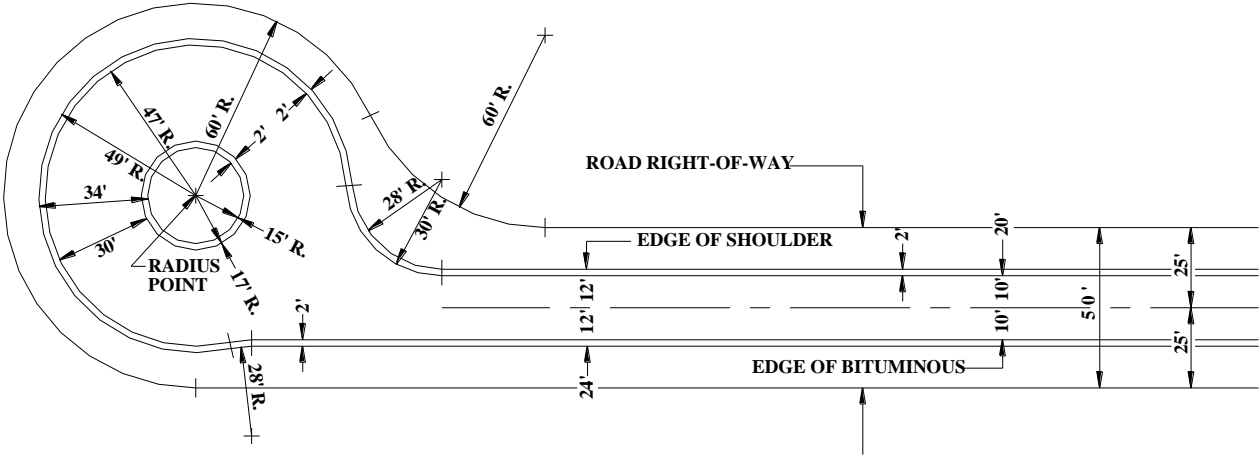
CONSERVATION DESIGN TYPICAL NON-ISLAND OFFSET CUL-DE-SAC



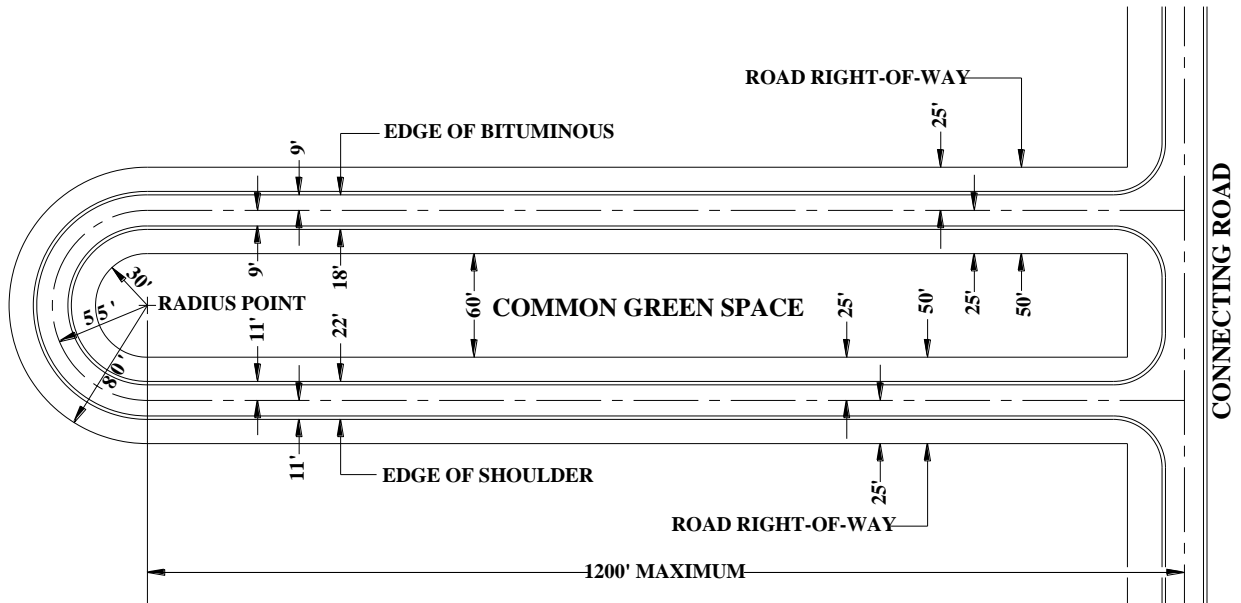
**CONSERVATION DESIGN
TYPICAL ISLAND CUL-DE-SAC**



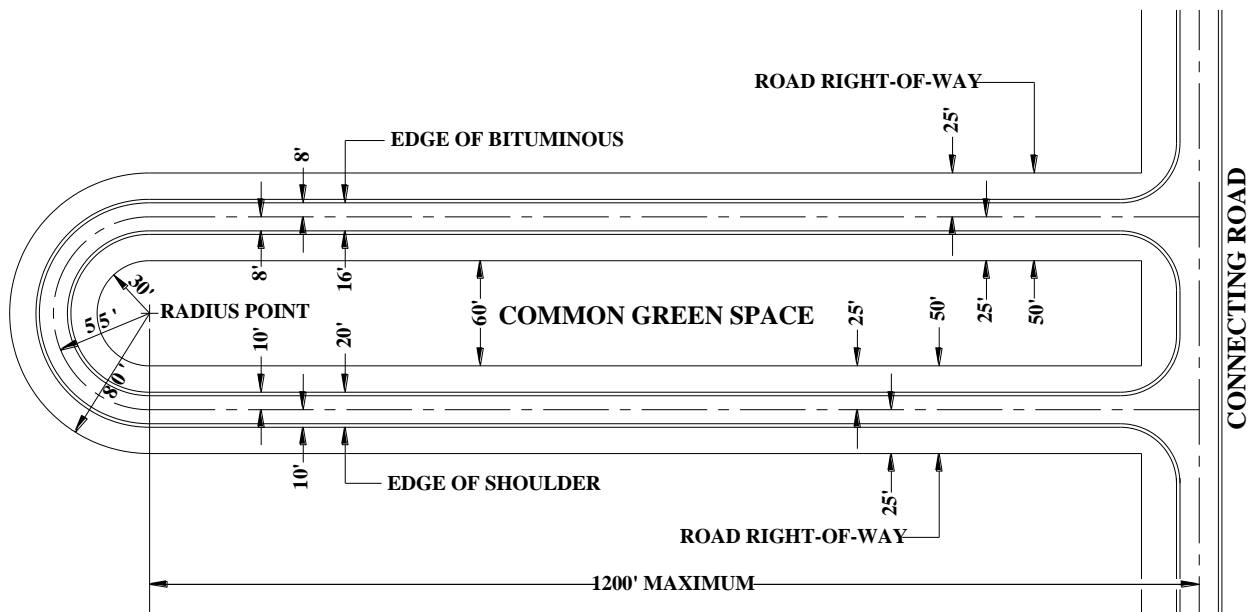
**CONSERVATION DESIGN
OFFSET ISLAND CUL-DE-SAC**



CONVENTIONAL DEVELOPMENT LOOP ROAD



CONSERVATION DESIGN LOOP ROAD



- c. As an alternative to the rural design and construction standards, urban design (curbed) roads may be used if designed and constructed in accordance with the “Wisconsin Department of Transportation Standard Specifications for Highway and Structure Construction”, the “Facilities Development Manual of the Wisconsin Department of Transportation” and the standards in the following table:

URBAN ROAD, LOOP & CUL-DE-SAC CONSTRUCTION STANDARDS					
Standards	Conventional Development		Conservation Design or Traditional Neighborhood	Conventional Development	
	Subcollector & Access Roads	One-Way Road	Access Road	Cul-de-sac without Island	Cul-de-sac or Loop Road with Island
Traffic Lanes	2	1	2	2	1
One-Way Counter Clockwise Movement	NA	NA	NA	NA	Yes
Vegetated Center Island	NA	NA	NA	No	Yes
R.O.W.	66'	50'	50'	60' radius	80' radius
Outside Pavement Radius, edge of pavement or face of curb	NA	NA	NA	49'	71'
Center Island Radius, edge of pavement or face of curb	NA	NA	NA	NA	48'
Road Width Before Gravel or Base Course	NA	NA	NA	53' radius	74' radius
Road Width After Gravel or Base Course	NA	NA	NA	51' radius	72' radius
Road Surface Paved	26'	20'	20'	49' radius	20' width
Slope Towards Centerline for 4 feet beyond high point of curb	2%	2%	2%	NA	2%
Compacted Sand Sub Base¹	12"	12"	12"	12"	12"
Compacted Base¹					
Crushed Limestone or	6"	6"	6"	6"	6"
Wisconsin Grade #2 Gravel	7"	7"	7"	7"	7"
Fill Slopes and Back Slopes	4:1	4:1	4:1	4:1	4:1
Minimum Radius of Curvature in ft. from centerline for deflections of 7° or more³	200	100	100	NA	100
Design Speed for Vertical Alignment	25 m.p.h.	25 m.p.h.	25 m.p.h.	25 m.p.h.	25 m.p.h.
Maximum Grade within 50' of the Center of the Intersection	2%	2%	2%	2%	2%
Curb and Gutter Required	Yes	Yes	Yes	Yes	Yes
Curb & Gutter Design²	2'	2'	2'	2'	2'
Corner Radii³	20'	20'	20'	20'	20'
Town Paving Standard	3" Lift	3" Lift	3" Lift	3" Lift	3" Lift

¹ Decomposable material shall not be used in construction.

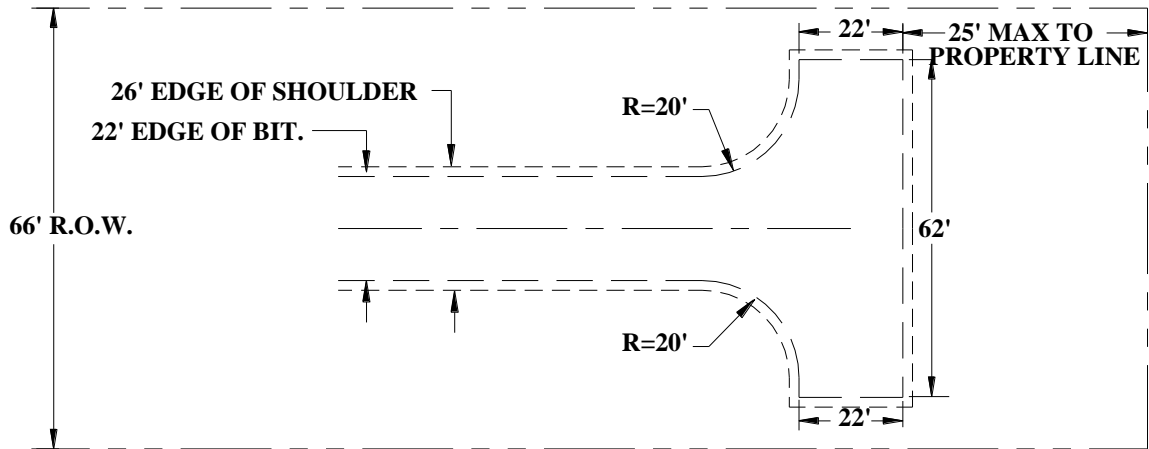
² Mountable or surmountable concrete curb optional.

³ See diagrams above.

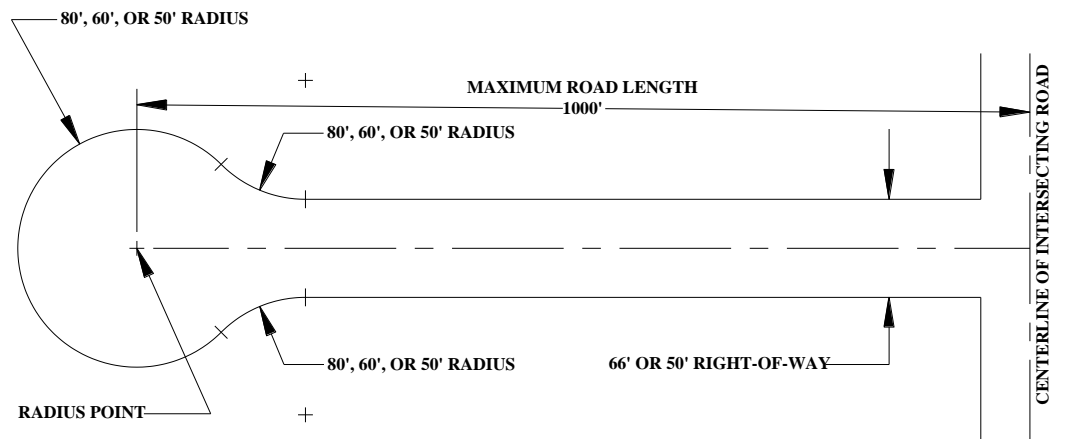
3. ROAD LAYOUT STANDARDS

- a. The **Town Board** shall examine the design of roads and driveway accesses to assure that lots are laid out in a way that will produce intersections, grades and other features satisfying the following standards:
 - 1) The number of intersections along arterial, collector and subcollector roads shall be held to a minimum. Wherever practicable, the distance between such intersections shall not be less than 1000 feet along these roads.
 - 2) Road intersection jogs with centerline offsets of less than 150 feet on existing or proposed roads are not allowed.
 - 3) Not more than two roads shall intersect at one point.
 - 4) The intersection angle of a driveway access to a road, and a road to a road, shall not be less than 75 degrees measured from the centerline of the road or driveway access to the right-of-way of the intersecting road or driveway access.
 - 5) The **Town Board** may require intersection vision clearances to the lot area within the right of way.
- b. Subdivision roads shall provide connection to existing or planned road extensions and adjacent unplatted property.
- c. All road layout design shall be consistent with any applicable policies adopted in the Town's comprehensive plan or official map adopted under Wisconsin Statute §66.1001 or §60.61 (2)(e).
- d. Proposed roads shall extend to the boundary lines of the lot, [parcel](#), or site being subdivided or developed unless prevented by topography or other physical conditions or unless, in the opinion of the **Town Board**, such extension is not necessary or desirable for the coordination of the layout of the land division or for the advantageous development of adjacent lands or Conservation Design Development.
- e. All arterial and collector roads shall be built to the boundary of the subdivision.
- f. All road extensions shall be constructed at the time of the development's initial road construction.
- g. Narrow strips of land between the road and the subdivision boundary (spite strips) shall not be permitted unless conditions under which the adjacent [parcel](#) can be connected to the road are established
- h. The vertical alignment of the centerline shall be based on the minimum safe stopping sight distance in accordance with the design standards of AASHTO.
- i. A permanent, platted dead-end road shall end in a cul-de-sac.
- j. The total length of a cul-de-sac road shall not exceed 1000 feet unless otherwise provided for in the town comprehensive plan or official map. The total length shall be measured from the centerline of the intersecting road to the radius point. The Town Board may allow a cul-de-sac road in Conservation Design Development to exceed 1000 feet if it is not possible to intersect with another road in the development without occupying a principal conservation area.
- k. Temporary termination of roads intended to be extended at a later date shall be accomplished with the construction of a temporary "T"-shaped turnabout contained within the road right-of-way. The T shall be constructed as shown in the figure below:

TYPICAL TEMPORARY TERMINATION "T" - SHAPED TURNABOUT



RIGHT-OF-WAY STANDARDS FOR CUL-DE-SAC ROADS



- l. A loop road shall not exceed 1,200 feet from the intersecting road centerline to the radius point in length unless otherwise provided for in the town comprehensive plan or official map.
- m. A one-way access road shall not exceed 2,400 feet in length unless otherwise provided for in the town comprehensive plan or official map. The length shall be measured from the centerline of each intersecting road.
- n. The planning, location and designations of roads in an area shall not allow the continuation of traffic from residential developments directly into commercial or industrial developments or vice versa. The **Town Board** may grant a waiver if no other option is available and appropriate buffering is provided.

- o. All lots in [major subdivisions](#) shall be served by new, interior public roads. An exception may be made by the **Town Board**.
- p. Subdivisions with 30 lots or more shall have two or more interconnected accesses onto a public road existing prior to the development of the subdivision unless otherwise provided for in the town comprehensive plan or official map. When counting lots under this provision, all existing and proposed lots shall be counted.
- q. Each lot shall have access to a public road either by frontage on a public road or by easement access approved by the **Town Board**.
- r. All accesses serving three or more lots or parcels shall be dedicated public roads.
- s. Any private road existing prior to the effective date of this ordinance that had served two or more lots or parcels shall be dedicated in its entirety if any additional lots or parcels will take access from the private road.

4. DRIVEWAY ACCESS STANDARDS WITHIN THE RIGHT-OF-WAY

- a. All driveway accesses that abut a proposed road shall be constructed through the right-of-way concurrent with the road construction.
- b. The maximum centerline slope of a road at a driveway location shall not exceed 6%, per the American Association of State Highway and Transportation Officials (AASHTO) intersection requirements for safety.
- c. The maximum slope of a driveway within the right-of-way shall be +/- 2%.

B. EROSION CONTROL & STORMWATER DESIGN FOR ROAD CONSTRUCTION

1. STANDARDS & SPECIFICATIONS

- a. Stormwater Management and Erosion and Sediment Control plans for road construction shall meet or exceed the design criteria, standards and specifications and [Best Management Practices](#) identified in b. below and in the following documents or their subsequent revisions:
 - 1) NR 151 Subchapters I, III and V.
 - 2) The Wisconsin DNR Stormwater Construction and Post Construction Technical Standards.
 - 3) The Wisconsin Department of Transportation Erosion Control Product Acceptability List.
 - 4) The Wisconsin Department of Transportation Standard Specifications for Highway and Structure Construction Manual.
 - 5) Wisconsin Department of Transportation Facilities Development Manual, Chapter 10.
 - 6) The United States Department of Agriculture Technical Guide 4.
- b. Stormwater management and erosion and sediment control plans shall be certified by a registered professional engineer.

C. SUPPLEMENTAL ROAD STANDARDS FOR COMMERCIAL OR INDUSTRIAL USE

1. STANDARDS

- a. Minimum road rights-of-way shall be 80 feet unless the right-of-way pre-exists adoption of this ordinance. In this case, the **Town Board** may authorize a reduced minimum right-of-way based on the criteria listed in i) below.

- b. Minimum pavement width shall be 24 feet.
- c. Minimum turn radii shall be sufficient to handle the size of vehicles likely to use the site.
- d. Where the subdivision and/or development of the area will likely involve multiple [parcels](#) and/or buildings, the design shall include frontage roads, shared driveway accesses or other means of reducing direct access to arterial roads.
- e. The subdivision review process shall include specification of the internal road network and internal driveway access arrangements to assure that spacing of access points, queuing distances, turn radii, and the like are appropriate to the volumes of traffic and types of vehicles and vehicular movement likely to be associated with the development.
- f. The **Town Board** shall designate a professional engineer to review road plans and submit comments.
- g. Roads shall be constructed and paved to meet designs approved by a professional engineer designated by the **Town Board**.
- h. The **Town Board** may require cross easements where commercial lots are side-by-side to allow linking of parking areas.
- i. Road standards for commercial or industrial developments may be increased by the **Town Board**. An increase of these standards will be based on relevant information such as town comprehensive plan or official map, driveway access widths, speed limit, number and types of vehicles using the road, parking availability, sound engineering judgment, and any other pertinent information.

D. CONSTRUCTION WITHIN SUBDIVISIONS

1. REQUIRED INSTALLATIONS

- a. Any road intersecting with a public road, and any road serving more than two lots, shall be improved by the subdivider, including necessary bridges, culverts and ditches, to standards established by this ordinance.
- b. All road and related stormwater improvements required by the approved preliminary plat and/or this ordinance, shall be completed and accepted by the **Town Board** prior to approval and filing of the final plat, final certified survey map, or prior to building or facility construction.

2. TIMEFRAME

- a. The installation of the required improvements or removal of existing features and temporary management structures will be identified in the construction plans.
- b. All required improvements shall be installed and completed in substantial conformance with the approved plans and as specified in the construction plans within one year following the effective date of the initial financial assurance.
- c. The Town may set provisions to address the failure to install required improvements within one year, such as penalties and the use of financial assurances to pay those penalties.
- d. If the required improvements are not installed within one year, then the **Town Board** may subject the subdivider to forfeitures and use the financial assurance to stabilize the site.

E. FINANCIAL ASSURANCE

1. PURPOSE

- a. A financial assurance shall be provided to ensure the proper construction, installation and maintenance of required roads; related stormwater management and erosion and sediment control, landscaping and removal of existing features; and temporary erosion and sediment control and stormwater management structures.

2. ASSURANCE

- a. The owner or the subdivider shall be responsible for providing the assurance.
- b. The nature and duration of the assurance shall be structured to achieve installation and maintenance without adding unnecessary costs to the responsible party.
- c. The **Town Board** may extend the time allowed for installation of an improvement for which the assurance has been provided.
- d. The financial assurances shall be required before the construction of required improvements and as a condition of the preliminary subdivision approval.
 - 1) The assurance shall be 120 percent of the estimated cost of installing an improvement.
 - 2) The assurance shall be valid until substantial completion of all required improvements and released by the **Town Board**.
 - 3) The assurance shall be retained upon completion of the required improvements for maintenance purposes.
 - a) The retained assurance shall be for a period not to exceed 2 years after final acceptance of an improvement.
 - b) In the event that other governmental agencies or public utilities obtain title to the improvement, or the improvement is covered by a maintenance agreement or other assurance to another governmental agency, no assurance shall be retained.

3. FORM

- a. The assurance shall be secured.
- b. The **Town Board** may select from a variety of secure means including, but not limited to, the following:
 - 1) An irrevocable letter of credit from a reputable bank or lending institution acceptable to the Town.
 - 2) Cash or an instrument readily convertible into cash.

F. RELEASE OF ASSURANCE

1. DECISION PROCESS

- a. Upon substantial completion of all required improvements, the responsible party shall notify the **Town Board** of the completion of and cost of the improvements in writing, by certified mail.
- b. The **Town Board**, in consultation with appropriate persons, shall inspect the improvements and shall authorize approval, partial approval, or rejection of such improvements.
- c. A statement of reasons for rejection and corrective action shall be provided in writing.
- d. The responsible party shall correct the condition specified by the stated corrective action within the time stated.

- e. If the corrective action is not completed within the specified time, the **Town Board** may utilize the assurance to complete the corrective action.
- f. If the **Town Board** takes no action to approve, partially approve, or reject the improvements within 45 days of receipt of the notice of substantial completion, the improvements shall be deemed to have been approved, and the responsible party and/or surety, if any, shall be released from the assurance for such improvements, less any amount retained under § **E.2.d.** above

2. PARTIAL APPROVAL OR DENIAL

- a. Where partial approval of the improvement is granted, the responsible party shall be released from liability under the assurance to the extent of the approval.
- b. At the Town Board's discretion and based on road inspections completed by a licensed engineer, the assurance may be released for construction but retained for paving for up to 2 years to allow for paving observation after a freeze/thaw cycle.
- c. If approval of the improvement is denied, the **Town Board** shall utilize the assurance to see that improvements are properly completed.

G. DEFINITIONS

1. INTERPRETATION

- a. For the purpose of administering and enforcing this ordinance, the terms or words used herein shall be interpreted as follows:
 - 1) Words used in the present tense include the future; in the singular include the plural and in the plural include the singular.
 - 2) The word "shall" is mandatory, not permissive.
 - 3) All distances, unless otherwise specified shall be measured horizontally.
 - 4) All definitions that refer to Wisconsin Statutes shall incorporate any revisions or amendments to statutory language.
 - 5) All definitions other than those referenced below shall be as the normal definition found in a standard dictionary.

2. DEFINITIONS

- a. **Bulb of Cul-de-sac:** The arc of the terminating radius, not including the reverse curves onto the tangents of the uniform road width.
- b. **Plat:** A map of a major subdivision prepared in accordance with Wisconsin Statutes § 236 and this ordinance.
- c. **Road:** A public way for vehicular traffic, this term includes highways.
- d. **Road Classification System:** Roads are classified based on many factors, including speed and volume of traffic. Road classifications fall into a four-category hierarchy. The classifications, in descending order are arterial, collector, subcollector and access roads (see diagram below). The Wisconsin Department of Transportation (WisDOT) and the St. Croix County Highway Department determine arterial and major and minor collector road status on their respective systems and the current functional classification will be used. The appropriate municipal jurisdiction, determines subcollector and access roads. Classifications are shown on the St. Croix County Official Functional Classification Road Map. Residential roads may fall into any of these classifications except arterial. The functional descriptions of each of these classifications follow:
 - 1) **Arterials:** Provide for rapid speed and movement of high volumes of traffic between areas. An arterial road should have no private accesses on it. Its function is to conduct traffic between communities and activity centers and to connect communities to major state and interstate highways. There are two types of arterials, principal arterials and minor arterials. Principal arterials are those highway corridors that have trip length and travel density characteristics of an interstate or interregional nature. Principal arterials, in general, serve all urban areas greater than 5,000 population and may include: interstate highways, freeways, expressways, four-lane divided highways and two-lane highways. Minor arterials are those highways, which in combination with principal arterials, serve cities, communities and other major traffic generators providing intra-regional and inter-area travels. Minor arterials may include four-lane divided highways, two-lane highways and county trunk highways.
 - 2) **Collectors:** Provide for moderate speed and movement of medium volumes of traffic and distribute traffic from arterial roads. There are two types of collector roads, major collectors and minor collectors. As the principal road within residential or commercial areas, these collectors carry relatively high traffic volumes and convey traffic from arterial roads to lower-order roads. A collector's function is to promote the free flow of traffic; as such, the roads should not have parking or private residential access. A collector's secondary function is to serve abutting land uses. Major collectors provide service to moderate sized communities and other intra-area traffic generators and link those generators to larger population centers and higher function highways. Minor collectors provide service to all remaining smaller communities and tie local traffic generators with the rural surrounding area. Minor collectors are spaced consistent with population density so as to collect traffic and bring all developed areas within a reasonable distance of a higher-order road.
 - 3)

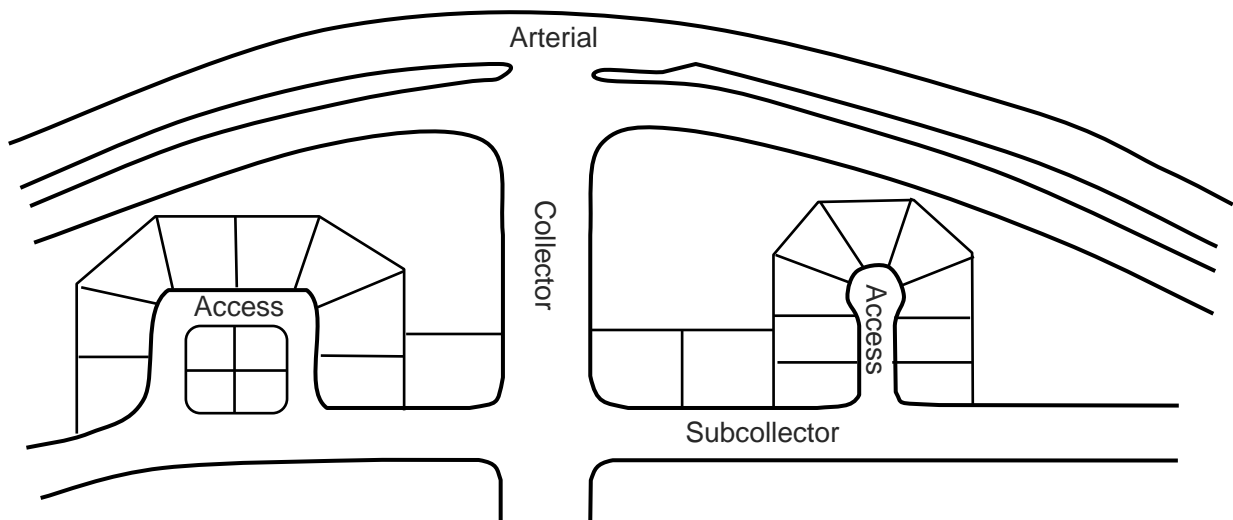
- 4) **Subcollectors:** Connect with access roads and convey traffic to major and minor collectors. Like an access road, a subcollector provides frontage and access to residential lots but also carries some through traffic to access roads. A subcollector is a relatively low-speed, low-volume road. Subcollectors include all roads not identified as major or minor arterials, major or minor collectors or access roads. Generally, subcollectors are all town roads in the original town road grid system and town roads identified as through roads or subcollectors on a town comprehensive plan or official map. Some roads in a subdivision may be subcollectors instead of access roads if the two ends of the road extend beyond the boundary of the subdivision and one end of the road eventually connects to another subcollector or higher order road.
- 5) **Access Roads:** Roads designed to conduct traffic between individual parcels or lots and higher order roads. Access roads provide for low-speeds and low volumes of traffic and convey traffic to subcollector, collector and arterial roads. As the lowest-order road in the hierarchy, the access road usually carries little through traffic and includes short roads, cul-de-sacs, and courts. Access roads include local roads within conventional subdivisions, local roads within Conservation Design Development subdivisions, cul-de-sacs, loop roads, lakeshore roads that existed prior to 1974, and roads not otherwise classified located in unincorporated hamlets, including Boardman, Burkhardt, Cylon, Emerald, Erin Corners, Forest, Hersey, Houlton, Huntington, Jewett, Johannesburg and New Centerville.

Cul-de-sac: Roads closed at one end with turn-arounds, not platted for extension.

Loop: Short, one-way through roads that turn back to the originating road.

Temporary Dead-end: Roads that terminate in a “T” turn-around and are platted for future extension.

Functional Classification System Diagram



June 3, 2021

2021-6-3-1

Town Road Design & Construction Review & Inspection Fees	
<i>Land Divisions – Major Subdivision, Plat, Certified Survey Map, etc.</i>	<i>All Fees Required with Preliminary Plat Submittal</i>
Road Design Review & Inspection	\$550 + \$60 per lot/outlot
Paving Review & Inspection	\$700
Storm Sewer Inspection (where applicable)	\$825
Curb and Gutter Design Inspection (where applicable)	\$825
Financial Assurance Road Construction (refundable based on performance)	120% of Improvements
Financial Assurance Road Paving (refundable based on performance)	120% of improvements

June 3, 2021 Township of Baldwin Board Meeting. Pages 323-324.

Don L. Johnson moved to adopt the Town of Baldwin Road Ordinance Number 2021-6-3-1 for construction standards of board approved driveways shared by two residents. This ordinance does not change the fact that major subdivisions are prohibited and that no additional public roads will be taken over by the town. The fee schedule can be changed at any time without changing the ordinance. Seconded by Joseph Kusilek. Carried by voice vote. Yes-3, No-0. Attested: Jim Harer, Clerk.

Town of Baldwin Road Ordinance